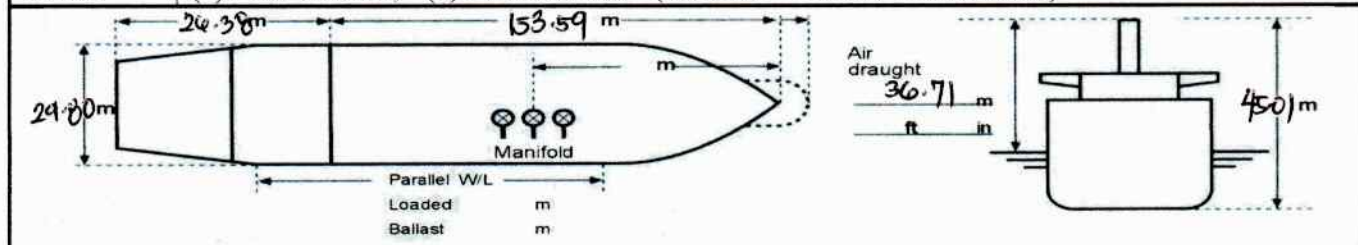


POS	PILOT CARD	Form Number	NAV -04
		Revision Number	08
		Revision Date	2024.08.31

SHIP PARTICULAR					
Ship's Name	PAN FORTUNE	Port	MARSDEN POINT, NEW ZEALAND	Date	16 FEBRUARY 2025
Call Sign	D7DA	Displacement	34,836.5 MT	DWT	26,179.5 MT
L.O.A	179.97 m	Breadth	29.80 m	Bulbous bow:	Yes
Draft FWD	7.60 M	Draft MID	7.95 M	Draft AFT	8.30 M
Freeboard	7.05 M	Over Head Clearance		Year Built	2016
Anchor	(P) : 11.5 Shackles, (S) : 11.5 Shackles (1 Shackle = 27.5 m / 15.04 Fathoms)				



MAIN ENGINE			
Type of Engine	MAN B&W 6S50ME Diesel	Max. Power	6,780 kw (HP)
Engine Critical RPM	55-66	Max. no of consecutive Engine starts	17
Time from F/Ahead to F/Astern:	379 Sec	Time Limit Astern:	Min
EPL Installation	YES	MCRlim	5480 kw (7348 HP)
EPL Release	NO	Time required for releasing EPL	5 Min

Manoeuvring Engine Order	RPM/Pitch	Speed(knots)	
		Laden	Ballast
EPL (Unreleased)	100.6	13.5	14.6
Full Ahead	76	11.4	12.5
Half Ahead	69	10.4	11.5
Slow Ahead	48	6.4	7.8
Dead Slow Ahead	35	2.9	4.5
Dead Slow Astern	35	Minimum Steering Speed : knots Minimum RPM: 22.97 , 1.1 knots	
Slow Astern	48		
Half Astern	69		
Full Astern	76	Astern Power: 40.6 % of Full Ahead	

STEERING			
No. of Propellers	1	Direction of turn	Right
Propeller Arrangement	FPP	Hard-over to Hard-over	22.8 sec
Rudder Angle for Neutral Effect	35 Degrees	Controllable pitch	No

EQUIPMENT CHECKED AND READY FOR USE						UKC	
Anchor	<input checked="" type="checkbox"/>	Compass incl. error	<input checked="" type="checkbox"/>	Nav. Lights	<input checked="" type="checkbox"/>	Min. UKC	
Echo Sounder	<input checked="" type="checkbox"/>	Compass Error 0.2E	<input checked="" type="checkbox"/>	ECDIS*	<input checked="" type="checkbox"/>	Speed	
Whistles*	<input checked="" type="checkbox"/>	VHF*	<input checked="" type="checkbox"/>	Daylight Signal*	<input checked="" type="checkbox"/>	SQUAT	
GPS	<input checked="" type="checkbox"/>	ROT/Pitch Indicator	<input checked="" type="checkbox"/>	Binoculars*	<input checked="" type="checkbox"/>	CB	0.782
RPM Indicator	<input checked="" type="checkbox"/>	Rudder Indicator	<input checked="" type="checkbox"/>			D/Ahead	0.32
Steering Gear	<input checked="" type="checkbox"/>	No. of Power units in use : 2				S/Ahead	0.95
X-Band Radar*	<input checked="" type="checkbox"/>	ARPA Yes/No	<input checked="" type="checkbox"/>	S-Band Radar*	<input checked="" type="checkbox"/>	H/Ahead	2.07
Engine Telegraphs : Set as ECR or Bridge(If required) control mode						F/Ahead	2.45
Speed Log	<input checked="" type="checkbox"/>	Doppler Yes/No	<input checked="" type="checkbox"/>	Speed : Water /Ground	Axis : Single /Dual		
EQUIPMENT OPEATIONAL DEFECTS							
- NIL -							

- Pilot, Master and bridge personnel share a responsibility for good communications and mutual understanding of the other's role for the safe conduct of the vessel in pilotage water.
- Master and OOW must immediately take whatever action is necessary until the vessel is in a safe position or situation when in any doubt as to the pilot's action or intention for ensuring the safety of the ship.
- Reference: IMO Resolution A.601(15) Provision and Display of maneuvering Information on board ships.
- * These navigation equipment can be operated by the pilot.

Pilot Name & Signature

Master of 'PAN FORTUNE'