
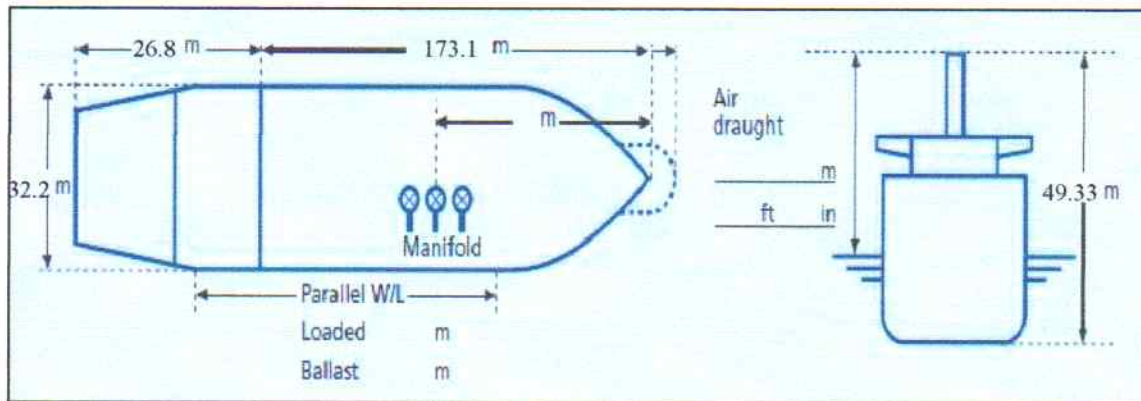


| | | | | |
|---|-------------------------------------|--|---|--------------|
|  | Bridge & Deck Procedures | | Doc No.: BDP-03-30A | |
| | BDP | | Revision: 01 | |
| | 03-00 Bridge Procedures | | Date: 30 Jul 2023 | |
| | App: 03-30A PILOT CARD | | Issued by: DP/COO Approved by: President | |
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| | | | | | | | | |
|-----|---------------|------------|-------|------|---------------|----------|-------|-----------|
| M.V | SOUTHERN STAR | Call Sign: | 3ELZ4 | Port | MARSDEN POINT | Voy: 110 | Date: | SEPT 2024 |
|-----|---------------|------------|-------|------|---------------|----------|-------|-----------|

Dimensions & Particulars

| | | | | | |
|-------------------|-------------|---------------|---------------|------------------------------------|---|
| Draught - Fwd | 6.60 Metres | LOA | 199.90 Meters | Year Built: | 2007 |
| Aft | 6.95 Metres | Beam | 32.20 Meters | Port Anchor | 12 Shackles |
| Mean | 6.78 Metres | Light Ship: | 9904.0 t | Starboard Anchor | 12 Shackles |
| Trim | 0.35 Metres | Displacement: | 32991 t | Effective ship speed range (knots) | <input checked="" type="checkbox"/> 5 kts-14kts |
| Freeboard :Metres | 15.8 | Deadweight: | 23087 t | Bulbous Bow: | <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No |




Main Engine & Manoeuvring Speeds

| | | | | | |
|------------------------------------|----------------------------------|--|--------------------|---------------------|--|
| Type of Engine | Mitsui Man B & W 6550MC (mark 6) | Maximum Power | 8580KW x 127 RPM | Critical RPM | 55-65 rpm |
| Restricted RPM on Low Sulphur Fuel | 117 | Max No. of Consecutive Main Engine Starts | Nos. | 15 | |
| No of Thrusters | N/A | Bow Thrusters: <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No | Power | N/A-KW/HP | Stern Thrusters: <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No |
| Engine Order | | Ahead RPM/Pitch | Loaded Speed (kts) | Ballast Speed (kts) | Astern RPM/Pitch |
| D. Slow | | 37 rpm | 4.4 kts | 4.9 kts | 37 rpm |
| Slow | | 54 rpm | 6.5 kts | 7.1 kts | 54 rpm |
| Half | | 72 rpm | 8.6 kts | 9.4 kts | 72 rpm |
| Full | | 87 rpm | 10.4 kts | 11.4 kts | 87 rpm |
| Engine Order Delay | 5 Sec | Full ahead to Full astern | 60 Sec | Time limit astern: | Hrs/Min |
| Minimum speed | 34RPM / kts | Minimum steering speed | 2.5 kts | Astern Power | 40% ahead |

Propulsion and Steering

| | | | | | |
|---|-------|--|---|------------------------|----------------|
| No. of Propellers: | 1 | Direction of turn: <input checked="" type="checkbox"/> Right <input type="checkbox"/> Left | <input checked="" type="checkbox"/> Fixed <input type="checkbox"/> Controllable Pitch | Propeller Arrangement: | GOOD |
| Number of Rudders on board | 1 | Type of Rudder(s) | Streamed Line Semi-Balanced Hanging Type | Maximum Rudder angle | 35Deg |
| Rudder angle for neutral effect | 0 Deg | Time taken for Hard-over to Hard-over (Refer point no. 6 below) | P to S | 12 Secs | S to P 12 Secs |
| Steering Characteristics and Idiosyncrasies | GOOD | | | | |

| | | |
|---|---|---|
|  | <p align="center">Bridge & Deck Procedures</p> <p align="center">BDP</p> <p align="center">03-00 Bridge Procedures</p> <p align="center">App: 03-30A PILOT CARD</p> | Doc No.: BDP-03-30A |
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Equipment Checked and Ready for Use / Remarks

| Equipment | Tick | Additional comments, remarks | | | |
|---|-------------------------------------|--|--|--|--|
| Anchors | <input checked="" type="checkbox"/> | Cleared Away | <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No | | |
| Gyro and Magnetic Compasses and repeaters | <input checked="" type="checkbox"/> | Compass Error 0. / W | | | |
| Speed log | <input checked="" type="checkbox"/> | Doppler: <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No | Speed: <input checked="" type="checkbox"/> Water <input type="checkbox"/> Ground | Axis: <input checked="" type="checkbox"/> Single <input type="checkbox"/> Dual | |
| Echo sounder | <input checked="" type="checkbox"/> | | | | |
| GNSS (GPS) | <input checked="" type="checkbox"/> | Type: FURUNO | | | |
| X-Band radar(s) | <input checked="" type="checkbox"/> | ARPA <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No | Range Set | 1.5 N.M. | |
| S-Band radar | <input checked="" type="checkbox"/> | ARPA <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No | Range Set | 3.0 N.M. | |
| VHF (including handheld) | <input checked="" type="checkbox"/> | Channels | 16 | 11 | |
| Steering gear | <input checked="" type="checkbox"/> | No. of power units in use | 2 MOTORS | | |
| Engine telegraph | <input checked="" type="checkbox"/> | | | | |
| Rudder / RPM // ROT indicators | <input checked="" type="checkbox"/> | Wheelhouse & Bridge Wings P/S | | | |
| Navigation Light and Failure Alarms | <input checked="" type="checkbox"/> | | | | |
| Flags and Shapes | <input checked="" type="checkbox"/> | | | | |
| Aldis Lamp | <input checked="" type="checkbox"/> | | | | |
| Whistle | <input checked="" type="checkbox"/> | | | | |
| GMDSS and Communication Equipment | <input checked="" type="checkbox"/> | | | | |
| AIS | <input checked="" type="checkbox"/> | | | | |
| BNWAS | <input checked="" type="checkbox"/> | | | | |
| VDR | <input checked="" type="checkbox"/> | | | | |
| Mooring Winches & Lines | <input checked="" type="checkbox"/> | | | | |


| ECDIS | | | | | | | |
|--|--------------|--|----------------|---|--|--|---|
| Make | FURUNO | Model | FMD 3300 | Type | FURUNO | Display settings | <input type="checkbox"/> base <input type="checkbox"/> standard <input type="checkbox"/> all <input checked="" type="checkbox"/> custom |
| Settings | Safety Depth | 10M | Safety Contour | 10M | Shallow Contour | 7M | Cross Track |
| | | | | | | | 50-185 M |
| | | | | | | | Look Ahead Value |
| | | | | | | | 370M |
| Wheel Over | | CPA/TCPA | | Guard zone (in minutes or degrees/distance) | | | |
| ENC Update Wk | 36 | ENC Update applied for Port and approaches | | | <input checked="" type="checkbox"/> Y <input type="checkbox"/> N | Local Warnings available/applied | |
| | | | | | | | <input checked="" type="checkbox"/> Y <input type="checkbox"/> N |
| Backup system (eg paper charts, full ECDIS, etc) | | | | FULL ECDIS | | Any non-standard configuration applied | |
| | | | | | | | <input type="checkbox"/> Y <input type="checkbox"/> N |

Equipment Operational Defects (if any): NONE

Other important details: ALL NAVIGATIONAL EQUIPMENTS CHECKED AND TESTED & ALL FOUND IN GOOD WORKING CONDITION.

Company UKC Policy

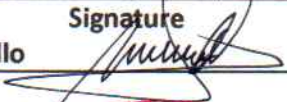
1) Ocean Passage – Vessel shall not enter waters with depth less than twice the maximum static draft.

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- 2) **Coastal Passage** – Vessel shall not enter waters with depth less than 120% of the maximum static draft
- 3) **Pilotage/Channel** etc - Vessel shall not enter waters with depth less than 110% of the maximum static draft.
- 4) In all above cases, a minimum 2 ft UKC shall be maintained at any given time after taking into account squat, rolling, pitching, heaving etc
- 5) At Berth or at SBM/CBM Mooring UKC at any time should be at least 1.5% of the ship's beam(except for NAABSA clause v/i) (Kindly refer to BDP-03-02)
- 6) Note: SOLAS, Ch II, Reg 29-3.2: Max time required from 35 deg either side to 30 deg on the other side should not be more than 28 sec.
- 7) In case a Port, Charterer and or local regulation has a UKC Policy which is stricter than the UKC policy as defined in BDP-03-02, the Master shall comply with the more stringentUKCpolicy among all in close consultation with the PIC.
- 8) Vessels that have installed power limiting devices (EPLs or permanent governors) must ensure that Pilot Card is updated with the maximum available power and Effective Ship Speed range as allowed by the EPL or governor installed.

Documentation Policyduring Pilotage and No Distraction Policy

1. Pilot to be briefed on Company No Distraction Policy. 2. Master and Bridge Team to do bare minimum paperwork under Pilotage and any other paperwork to be pursued after berthing with exception to information exchange and other Port Entry related documents

| | | | |
|-------------------------|---|---------------------|------------------|
| Master's Name | Signature | Pilot's Name | Signature |
| Capt. Frederick E. Ello |  | | |

